

ALLOUEZ TRANSPORTATION ALTERNATIVES PROJECT APPLICATION
Safe Routes to School Study—Webster School Project

The following key points are intended to provide background on the proposed Webster School SRTS sidewalk and school crossings project.

- WisDOT Transportation Alternatives Program (Safe Routes to School) is part of the federal and state efforts to promote biking and walking. SRTS projects are a priority because of the positive impact on youth to promote walking at an early age.
- Allouez has initiated an effort to add bike and pedestrian facilities in selected routes to provide some routes for local pedestrian and bicycle travel. These routes are established in the new Safe Routes to School Plan and include the recent Greene Avenue sidewalk, sidewalk included in the reconstruction of east Greene Avenue, the Heritage Hill trail, the Hoffman Road bike and sidewalk additions, and other planned routes in key areas. This effort is intended to provide a few selected routes for resident use.
- Funds have been allocated to the Green Bay Metro Planning Organization at \$600K for this round of project funding. Projects selected by the MPO are subject to WisDOT evaluation and final selection in competition with other projects statewide. Funds not used locally will probably go to other statewide projects.
- Allouez submitted a SRTS project application several years ago. It was not funded in part because it was too large and it was before a comprehensive village SRTS plan was prepared. The current application is a lower cost reduced scope project focused on the highest priority in the village SRTS plan.
- The Webster School area is very congested during school hours, and several improvements are planned to help alleviate congestion. This is in addition to the SRTS project. These include some no parking areas along the streets, implementing time limited parking at drop-off points, improving Jourdain Lane for ease of turn-around and parking stalls, and converting some lot parking to drop-off only. These are in conjunction with the school.
- The Webster SRTS sidewalks will provide auto drop-off areas on Woodrow and Brookridge which should help reduce congestion. The sidewalks should also reduce some traffic congestion due to more students walking to school.
- The proposed project must be approved by the MPO board. The project is then submitted to WisDOT for final review for compliance with all requirements.

- The local project cost portion will be about \$100,000 and is a 2015/2016 construction project. The design will be locally funded and a large part of the design completed by the village engineering department. The project is a reimbursed project which means it is funded locally and when completed the state share will be reimbursed.

Figure 5-1: Existing & Proposed Pedestrian Facilities for Allouez

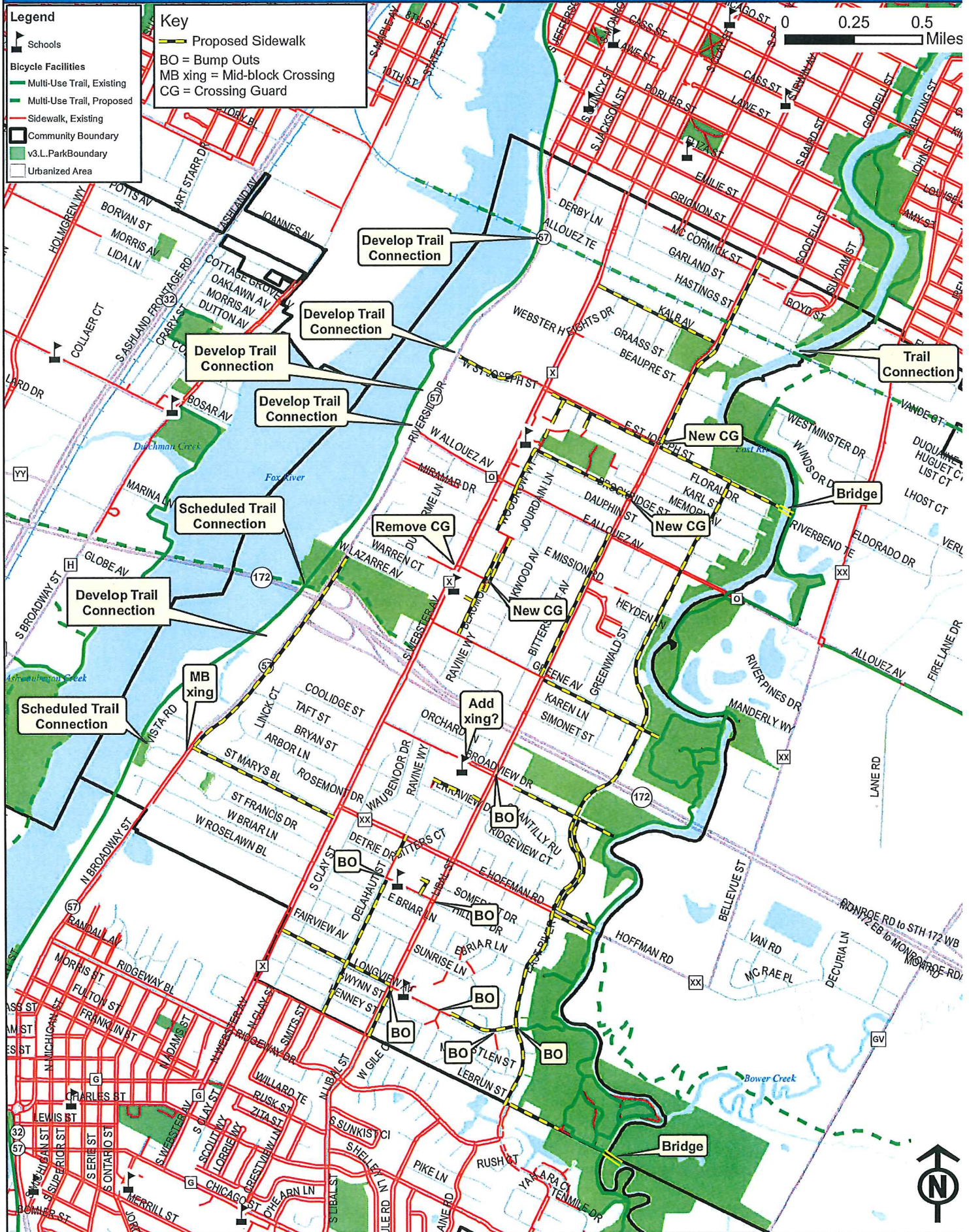
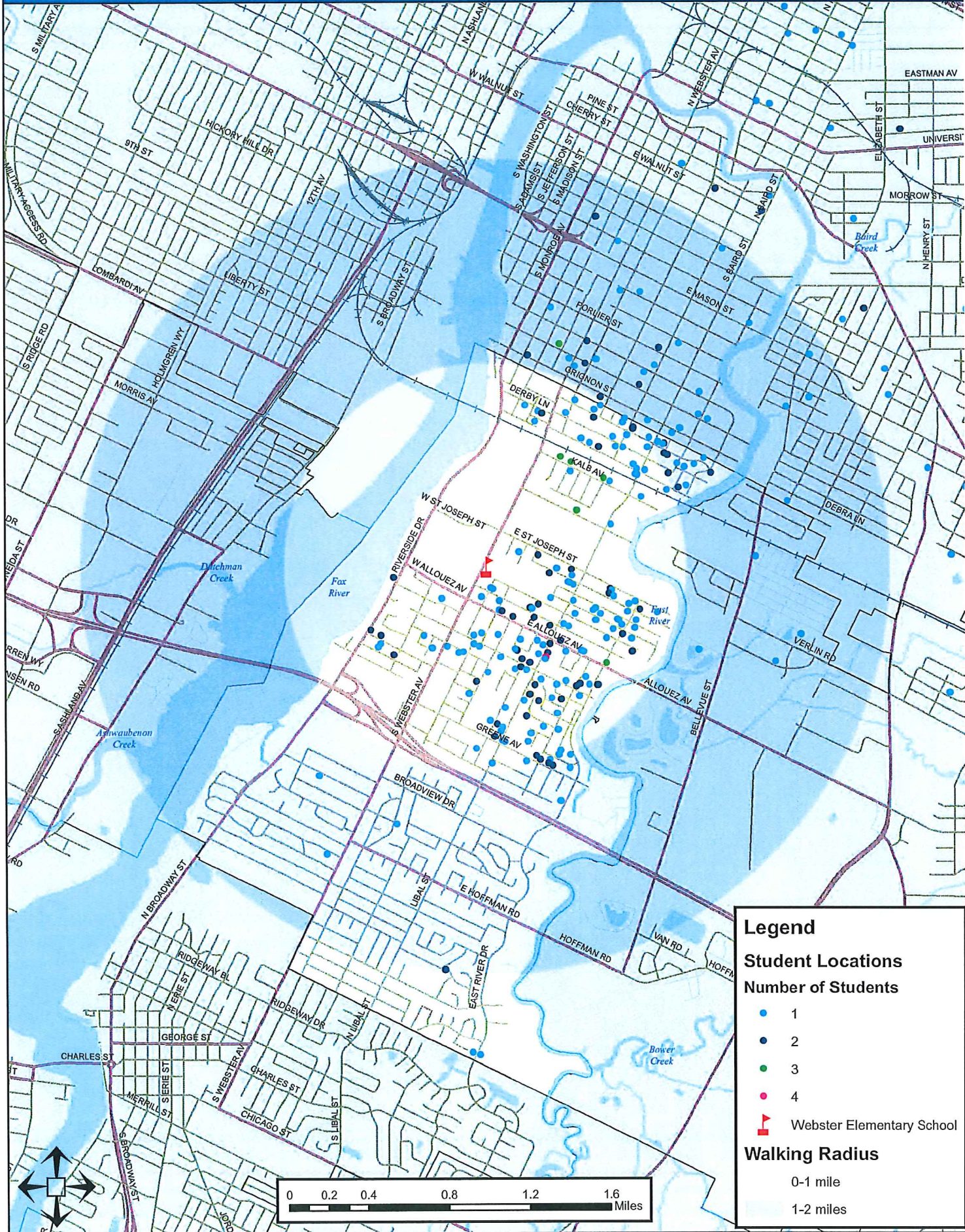


Figure 3-1: Webster Elementary School Student Locations (2011)



Proposed Improvements for Webster Elementary School Area



Reasons for the Webster Elementary School Engineering Recommendations

1. Add a sidewalk to the east side of Libal Street between the existing sidewalk and the north village boundary. The student address information collected during the planning process indicates that many Webster students live east of Libal Street in the northern portion of the village. However, the only sidewalk that these students can use to reach Webster School is on the west side of Libal Street. This forces students to cross Libal at uncontrolled intersections to reach the sidewalk, which is likely one of the reasons why students were not seen walking or biking to school during the Webster pedestrian/bicycling audit. Adding a sidewalk on the east side of Libal Street will provide a safe place for the students who currently walk and encourage additional students to walk to school. This sidewalk segment will also provide a safe and continuous pedestrian connection for all Allouez residents between the Libal sidewalk north of St. Joseph Street and the sidewalk that is currently in place on the east side of Baird Street in Green Bay.

2. Add a sidewalk to the south side of St. Joseph Street between East River Drive and Webster Avenue. Although a sidewalk already exists on the north side of St. Joseph Street, this sidewalk does not provide a safe route to Webster School for the many students who live north and east of the school. A sidewalk on the south side of St. Joseph Street will complement the other sidewalks recommended for north of the school and will provide the pedestrian link necessary for many parents and guardians to allow their children to walk and bike to school. This sidewalk segment will also provide a safe and continuous pedestrian connection for all Allouez residents between Webster Avenue and the sidewalk recommended for the west side of East River Drive.

3. Add a crossing guard at the St. Joseph Street/Libal Street intersection and study the intersection to identify the safest and most accessible traffic control option. The recommended Libal Street and St. Joseph Street sidewalks are important components of a safe pedestrian network north of Webster School, but it is just as important to ensure that students can safely pass through the St. Joseph/Libal intersection once they reach it. Adding a crossing guard at this two-way stop controlled intersection will enable students to cross the streets safely and provide the protection necessary for many parents and guardians to allow their children to walk and bike to school. This intersection should also be studied to identify a long-term traffic control method that maximizes safety, efficiency, and multimodal accessibility.

4 & 5. Add sidewalks on the east side of Woodrow Way and the west side of Schroeder Lane. These sidewalks should be added to provide safe places for students to walk between the new St. Joseph Street sidewalk and the school property.

6 & 7. Add sidewalks along the west side of Woodrow Way and the north side of Brookridge Street. The student address information collected during the planning process found that many Webster students live east and southeast of the school, and all of these students live within convenient walking distance. However, the streets that access the school and Webster Park from the south and east of Libal Street do not have sidewalks, so students who choose to walk to school are forced to walk in the streets while avoiding parked and moving vehicles. Adding sidewalks on the west side of Woodrow Way and north side of Brookridge Street will provide a safe place for students who currently walk and encourage additional students to walk to school. The new

sidewalks will also make these sections of Woodrow and Brookridge safe and convenient pick-up and drop-off areas for students who have to be driven to and from school, which could reduce traffic congestion at the current Woodrow Way pick-up/drop-off point behind the school.

8. Add bump-outs and a crossing guard at the Brookridge Street/Libal Street intersection. The bump-outs and crossing guard at this intersection will complement the sidewalk recommended for the north side of Brookridge Street by providing the protection necessary for many parents and guardians to allow their children to walk and bike to school. However, an engineering study should be completed before these bump-outs are built to determine their impacts on intersection turning movements and bicycle traffic on Libal Street.

